

US 82 Roadway Project

Artesia to Lovington

NMDOT Recommendations

The two leading alternatives for US 82 were evaluated in detail. The evaluation considered the performance merits, engineering feasibility, cost, and impacts of each alternative.

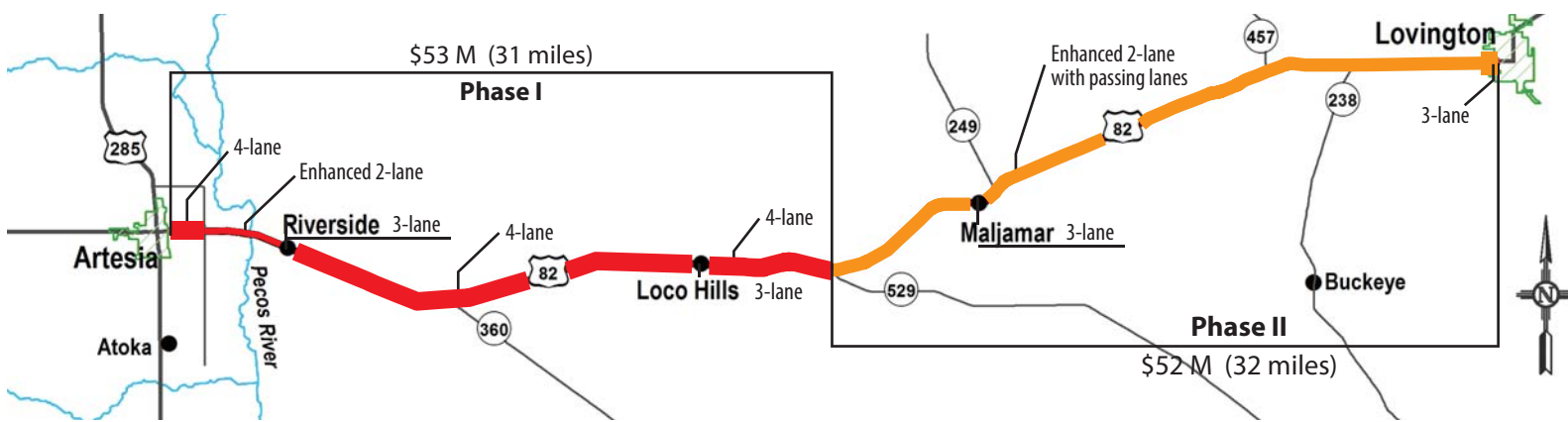
Primary Evaluation Metrics Used to Evaluate Alternatives

- Performance** – how each alternative improves the conditions within the project area that affect safety (i.e., passing, turns, etc.)
- Constructability** – the ability to reconstruct the highway without excessive disruption to highway users
- Affordability** – the cost of each alternative and the funding available to implement the alternative
- Right-of-Way Needs** – the amount, type, ownership, and presence of improvements on the land needed to implement each alternative
- Access** – how well each alternative provides safe access to the properties along US 82
- Impacts** – the impacts of each alternative on businesses, homes, utilities, and environmental resources



The Recommended Alternative

Based on the evaluation, Alternative 1 is recommended by the NMDOT Project Team. This alternative provides an affordable solution to the problems affecting this highway. However, at a cost over \$100M, implementation of this project will require more than one phase.

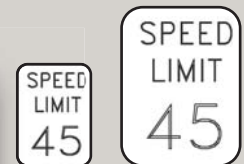


Key Design Features of Alternative 1

- Artesia to Haldeman Road** – four-lane section with auxiliary lanes at Haldeman
- Haldeman Road to east of Pecos Bridge** – two lane section with ten-foot shoulders
- East of Pecos Bridge through Riverside** – three lane section (two travel lanes and center turn lane with ten-foot shoulders)
- Riverside to NM 529** – four-lane with flush median with eight-foot shoulders
- NM 529 to Lovington** – two-lane section with eight-foot shoulders; passing lanes at approximate six mile intervals
- Loco Hills and Maljamar** – three-lane section (two travel lanes and center turn lane with 10-foot shoulders)
- Approach into Lovington** – three-lane section with 8-foot shoulders

Additional Features

- Posted Speed – 65 mph in mainline areas / 45 mph in developed areas and communities
- Radar speed signs, improved signage, and medians at community approaches
- Median and shoulder rumble strips



Radar speed signs and oversized signs provide greater visibility for drivers.

Phasing Approach

Because of funding limitations, the reconstruction of US 82 will be done in two phases:

- Phase 1** – Artesia to NM 529 (31 miles). This phase was given first priority because of its higher traffic volume and greater number and severity of crashes.
- Phase 2** – NM 529 to Lovington (32 miles). This phase will be constructed when the needed funding can be secured.



Reconstruction of US 82 will be completed in two phases.

Next Steps

Several steps are still necessary before construction can begin:

- Early September 2015:** Selection of Preferred Alternative – If comments from the general public and affected businesses and landowners support the recommended alternative, the NMDOT will make any refinements needed and proceed to the design phase.
- September to December 2015:** Preliminary Design and Environmental Approvals – Preparation of preliminary design plans, access changes, right-of-way needs, and environmental documents will be completed.
- December 2015 to September 2016:** Final Construction Plans, Right-of-Way Acquisition, and Bid Letting – Final construction plans for bid will be prepared and right-of acquisition will occur.
- Late 2016 and 2017:** Construction Underway

Project Phase	2015					2016											
	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sept	Oct	Nov	Dec
Selection of Preferred Alternative	[Yellow bar]																
Preliminary Design and Environmental Approvals	[Yellow bar]																
Final Design, R/W Acquisition, and Bid Letting	[Yellow bar]																
Construction Begins - late 2016/early 2017						[Yellow bar]											

