




**SUBJECT:** Infrastructure Design Directive  
IDD-2018-05  
Pavement Taper Detail

**DATE:** April 24, 2018

**TO:** Office of Infrastructure  
District Offices  
Transportation Design Community

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**FILE REFERENCE:**  
PSESHARE:Design Directives

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The following IDD shall be used to ensure consistency in the development of all projects to be let by NMDOT in relation to Pavement Taper Details. The attached directive establishes the NMDOT's policy in regards to the design and construction of surfacing tapers that apply to projects being designed and let out to bid under the NMDOT's project development process. This directive does not apply to maintenance projects.

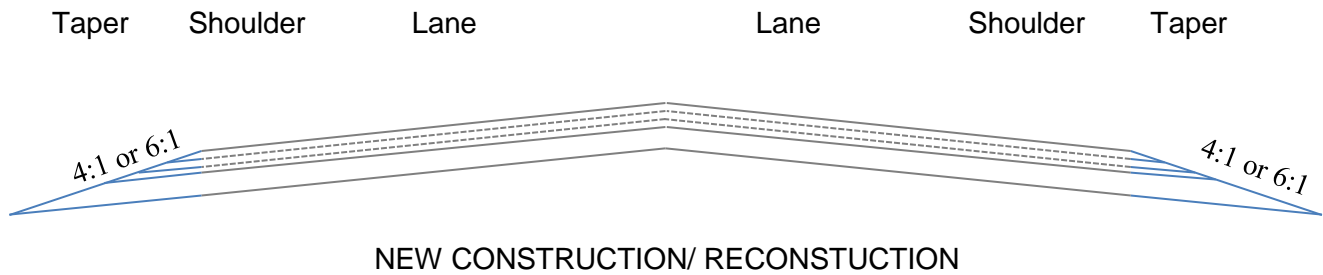
# NMDOT

## Pavement Taper Detail

The following Design Directive provides guidance to the PDE/designer regarding the NMDOT's policy for pavement taper details that apply to projects being designed and let out to bid under NMDOT's project development process. This directive does not apply to maintenance projects. Any variation to this Design Directive will require a Design Variance.

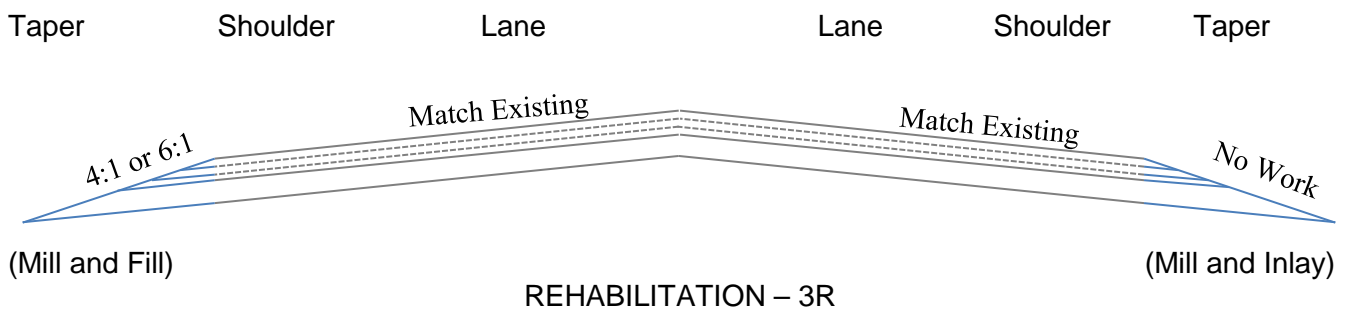
### New Construction/Reconstruction – 4R

Pavement surfacing tapers on all new construction and reconstruction projects will utilize a 6:1 surfacing taper when there are no constraints in regards to right of way, the environment or funding. Otherwise a 4:1 taper surfacing taper shall be utilized in these cases to minimize impacts.



### Rehabilitation – 3R:

Pavement surfacing tapers on Mill and Inlay projects are typically not affected as this process does not encroach into the taper area and therefore the existing surfacing taper will not be modified. Pavement surfacing tapers on 3R Construction and Mill and Fill projects that do encroach into the surfacing taper, will utilize a 6:1 surfacing taper when there are no constraints in regards to right of way, the environment or funding. Otherwise a 4:1 surfacing taper shall be utilized in these cases to minimize impacts.



Note: This Design Directive is not intended to preclude the need for the engineer/designer to assess clear zone and side slope treatments beyond the surfacing taper in accordance with the AASHTO Roadside Design Guide.